

DIVISION 30 – DOWNTOWN NORTHWEST DISTRICT

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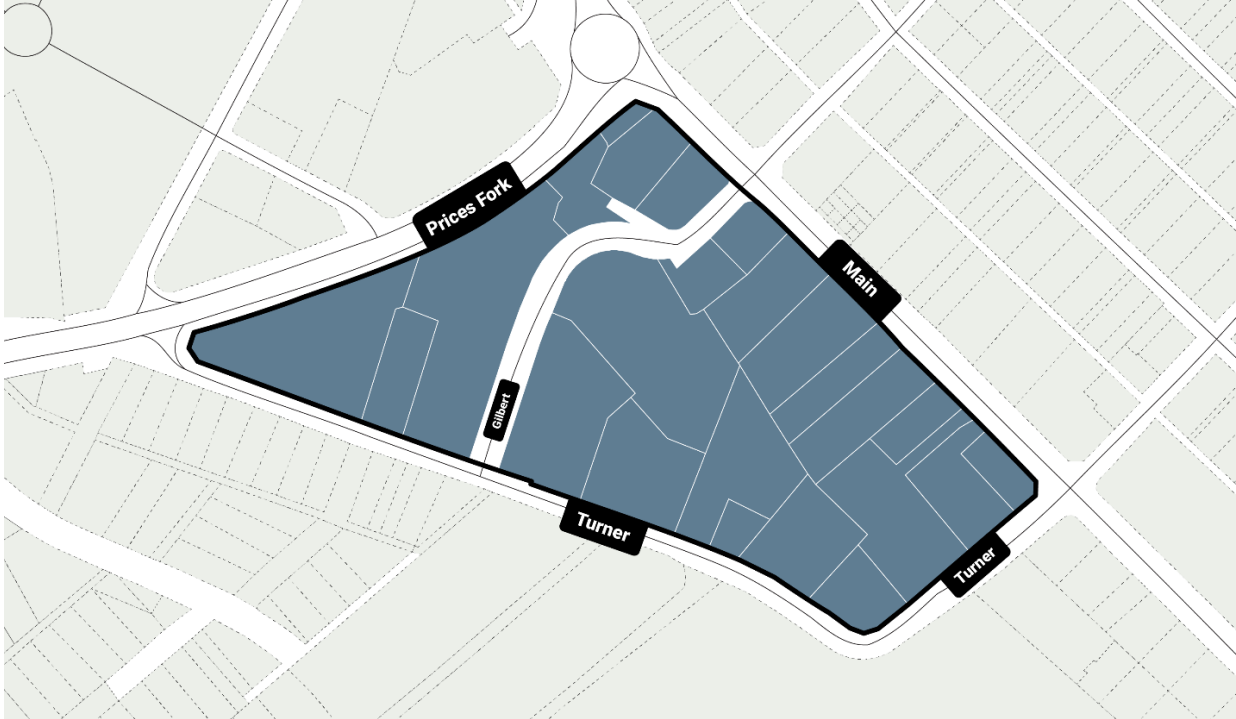
Sec. 3500 – Purpose

The Downtown Northwest District (DTNW) is a floating zone within the Downtown Commercial District that is established to provide an optional set of standards allowing for the creation of a dense, urban, well-connected, mixed-use development pattern within the triangle formed by North Main Street, Prices Fork Road, and Turner Street, commonly referred to as “Downtown Northwest,” as identified in the 2019 Downtown Strategic Plan. The DTNW District supports the creation of a vibrant, accessible, and connected urban environment through standards that address the use, scale, building placement, form, and design of development occurring within this area. District standards, applied as a whole, provide flexibility for new development to leverage density, height, and a mixture of permitted uses in exchange for high-quality design, pedestrian scale and orientation, bicycle friendliness, and sensitivity to the established character of the adjacent North Main Street corridor as well as the historic St. Luke and Odd Fellows Hall, located within the district boundary.

Sec. 3501 – Applicability

(a) The Downtown Northwest District may be applied to properties in the geographic area bounded by Turner Street NW, North Main Street, and Prices Fork Road as shown in the “Downtown Northwest District Applicability” graphic set forth below.

DOWNTOWN NORTHWEST DISTRICT APPLICABILITY



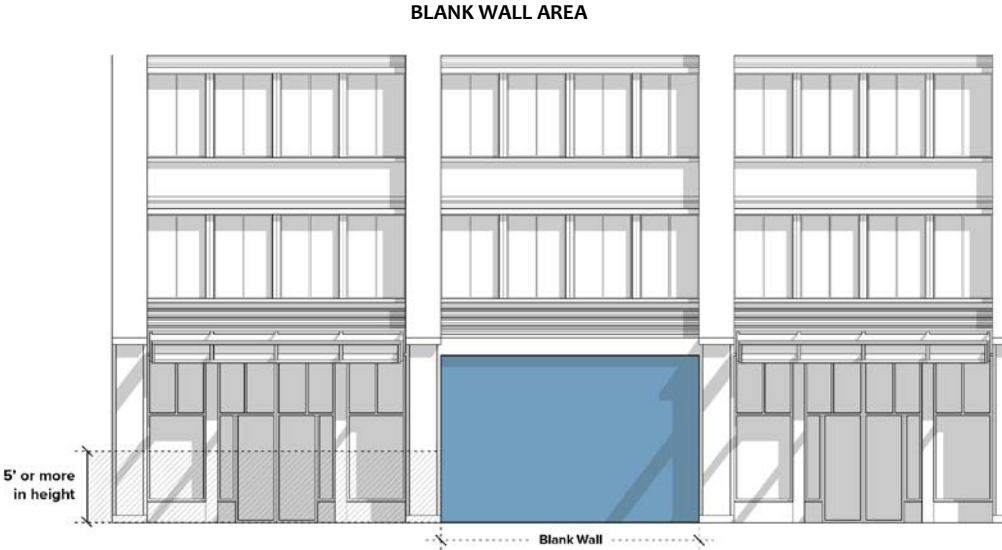
(b) No rezoning shall be approved which would result in the creation of a DTNW District of less than 3.5 acres in area, or less than 300 feet of contiguous frontage upon at least one public street.

Sec. 3502 – Definitions Applicable in the DTNW District

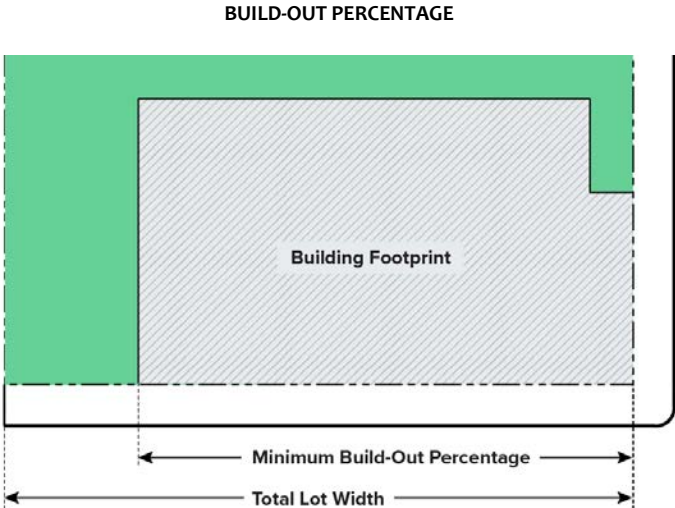
For the purposes of administering the DTNW District standards, the following definitions are established. In the case of a conflict, the definitions established in this section shall control within the DTNW District.

Bicycle Parking, Long Term. Bicycle parking spaces where bicycles can be stored within a safe and weatherproof storage area for long periods of time.

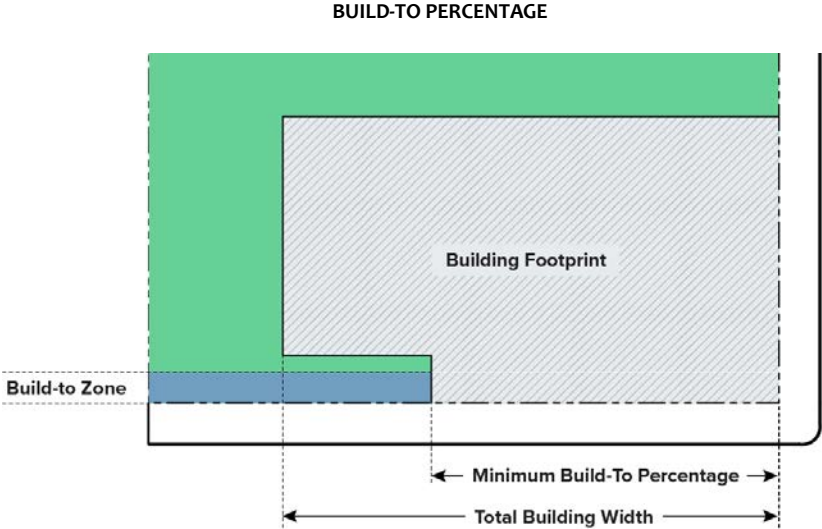
Blank Wall Area. The horizontal linear dimension of contiguous building wall that does not contain windows, doors, or decorative elements such as banding, medallions, artwork such as murals and mosaics, change in wall plane of at least three inches, or other architectural or material embellishment. Any wall less than five feet in height shall not be considered a blank wall.



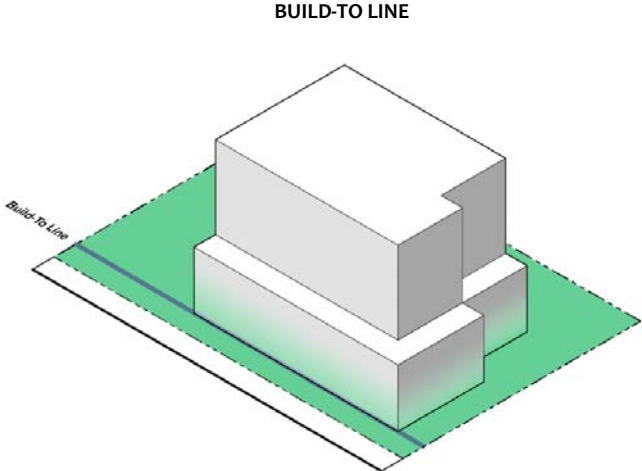
Build-Out Percentage. Build-out percentage specifies the minimum length, established as a percentage of total lot width, that shall be occupied by a building, within or beyond the build-to zone.



Build-To Percentage. Build-to percentage specifies the percentage of the total building width that shall be located within the required build-to zone.

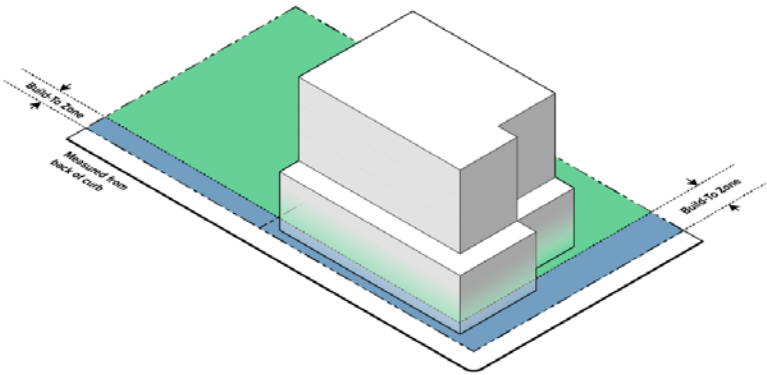


Build-To Line. A build-to line is a line on a lot establishing the required location of the front building line.



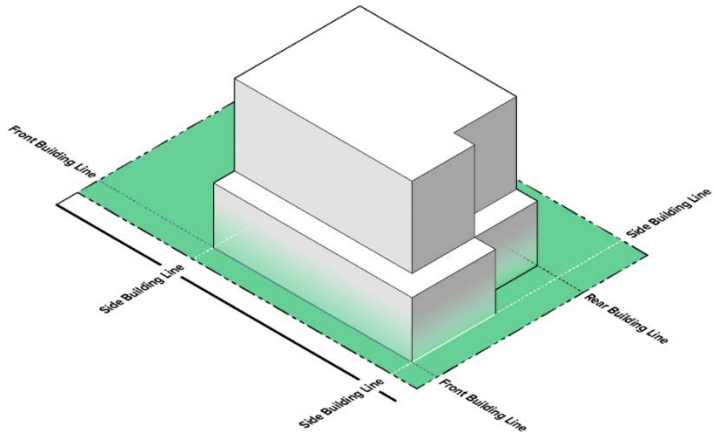
Build-To Zone. A build-to zone is the area on a lot, measured perpendicular to the front lot line and containing a minimum and maximum dimension, within which the front building line shall be located.

BUILD-TO ZONE



Building Line. A building line is the horizontal line indicating the primary edge of a building closest to the front, side, and rear property lines and extending from the physical edge of the building to the boundaries of the property.

BUILDING LINE

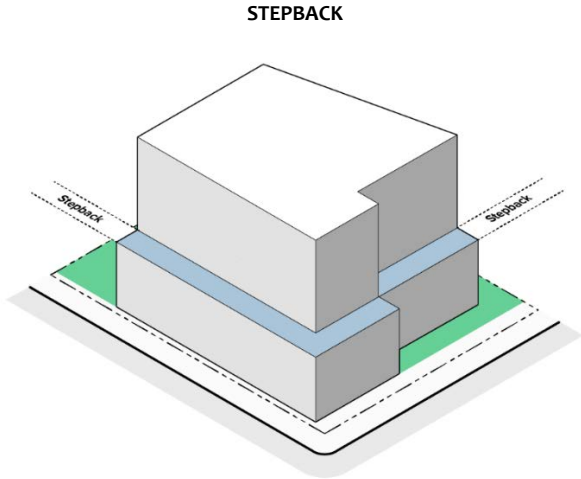


Expression Lines. Expression lines are those elements of a building façade that convey or express its structure, including but not limited to cornices, brick courses, banding, fenestration, columns, and bays.

EXPRESSION LINES



Stepback. Stepback is the additional distance that upper stories shall be recessed or set back from the façade of the stories below.



Transparency. The required amount of fenestration as a percentage of the specified façade area. Doors are included in ground floor transparency when such doors are designed with glass or other transparent materials. Garage entrances shall not be included in ground floor transparency.

- (a) The following do not meet the ground floor or upper floor transparency requirements and do not count in meeting the standard:
 - (1) Windows with shadowboxes on the interior
 - (2) Glass block
 - (3) Printed window film, signs, or posters affixed to the inside or outside of windows, regardless of whether such elements allow views into or out of the building.
 - (4) Mirrored glass on the ground floor.

Sec. 3503 – Permitted Uses

- (a) Table 3503-1: Downtown Northwest District Uses indicates the uses and structures permitted within the DTNW District, subject to all other applicable requirements contained in this Ordinance.
 - (1) Where a use is permitted by right, it is indicated as “P” within the table.
 - (2) Where a use is permitted with the approval of a Conditional Use Permit, it is indicated as “CUP” within the table. Uses permitted with a Conditional Use Permit are subject to the standards and procedures of Article 1, Division 8 of this Ordinance. In addition to Article 1, Division 8 of this Ordinance, approval of a conditional use within the DTNW District is subject to evaluation against the purpose and intent of the DTNW District relative to scale, orientation, context, and design quality.
- (b) Use standards for each of the uses allowed in the DTNW District are detailed in the column titled “DTNW District Use Standards.”
- (c) The use and design standards of Article 24 of this Ordinance do not apply to uses within the DTNW District unless specifically indicated otherwise. In the case of a conflict where the standards of Article 4 apply to a use in the DTNW District, the stricter standard shall control.

Table 3503-1: Downtown Northwest District Uses		
Use	P/CUP	DTNW District Use Standards
Residential		

Home Occupation	P	
Multifamily Dwelling <i>above the ground floor</i>	P	
Multifamily Dwelling <i>on the ground floor</i>	P	Not permitted on any frontage abutting North Main Street, or within 150 feet of the intersection of North Main Street and Turner Street NW. This does not apply to entries, lobbies, or common areas associated with any multifamily dwelling.
Civic		
Administrative Services <i>above the ground floor</i>	P	
Administrative Services <i>on the ground floor</i>	P	Not permitted on any frontage abutting North Main Street. Limited to 5,000sf GFA.
Community Recreation	P	
Club	CUP	
Cultural Services	P	
Day Care Center	P	
Educational Facilities, Primary/Secondary	CUP	
Home for Adults	P	
Post Office	P	
Public Assembly	CUP	Limited to 10,000sf GFA
Public Parks and Recreational Areas	P	
Public Recreation Assembly	P	
Religious Assembly	CUP	
Safety Services	P	
Shelter	P	
Utility Services, Minor	P	
Office		
Financial Institutions <i>without drive-through</i>	P	
Financial Institutions <i>with drive-through</i>	CUP	Subject to the standards of Sec. 350 .
General Office <i>above the ground floor</i>	P	
General Office <i>on the ground floor</i>	P	Not permitted on any frontage abutting North Main Street. Limited to 5,000sf GFA.
Medical Office <i>above the ground floor</i>	P	
Medical Office <i>on the ground floor</i>	P	Not permitted on any frontage abutting North Main Street. Limited to 5,000sf GFA.
Temporary offices for the management and support of a campaign for political office by a specific candidate or candidates (as defined by Code of Virginia § 24.2-101)	P	<p>1. Prior to occupancy of the office space, the campaign shall register and obtain a permit from the Administrator, which shall be valid for a period of not more than six calendar months and may not be renewed. Use of ground floor office space in the Downtown Commercial District for more than six (6) months requires a conditional use permit.</p> <p>2. Each permit shall specify the location of the temporary office, the dates of occupancy by the campaign and all necessary contact information.</p>
Commercial		
Brewpub	P	
Clinic	CUP	
Commercial Indoor Amusement	P	
Commercial Indoor Entertainment	CUP	
Commercial Indoor Sports and Recreation	CUP	
Communication Services <i>above the ground floor</i>	P	

Communication Services <i>on the ground floor</i>	P	Not permitted on any frontage abutting North Main Street. Limited to 5,000sf GFA.
Consumer Repair Services	P	
Dance Hall	CUP	
Grocery Store (25,000sf GFA or less)	P	
Grocery Store (Larger than 25,000sf GFA)	CUP	
Hotel/Motel/Motor Lodge	CUP	Subject to the standards of Section 4528.2
Neighborhood Convenience Store	P	
Outpatient Substance Abuse Treatment Center	CUP	
Parking Facility (Structured Only)	P	Allowed on lots fronting North Main Street only when other permitted commercial uses occupy the North Main Street frontage.
Personal Improvement Services	P	
Personal Services	P	
Public Farmers Market	P	Subject to the standards of Section 4548
Restaurant, Drive-in	CUP	Subject to the standards of Section 4552, as well as the drive through facility location and design standards of Section 350 .
Restaurant, Fast Food	P	Subject to the standards of Section 4552.1(a)
Restaurant, General	P	Subject to the standards of Section 4553(a)
Restaurant, Small	P	Subject to the standards of Section 4555(a)(2)
Retail Sales	P	
Specialty Shop	P	
Studio, Fine Arts	P	
Veterinary Hospital/Clinic	P	No overnight boarding is permitted.
Industrial		
Laboratory	CUP	Only allowed above the ground floor when fronting North Main Street. Limited to 5,000sf GFA.
Research and Development	CUP	Only allowed above the ground floor when fronting North Main Street. Limited to 5,000sf GFA.
Miscellaneous		
Accessory Structures	P	<ol style="list-style-type: none"> 1. Accessory structures shall be permitted in association with a principal structure, provided that the area of the accessory structure shall be 25 percent or less of the gross floor area of the ground floor of the principal structure. 2. Accessory structures shall be allowed only on the same lot as the principal use or structure, or adjacent lot under common ownership, provided that access is from the lot on which the principal use or structure is located. 3. Accessory structures are not permitted within any required build-to zone, or between a principal structure and any street. 4. Retail and shipping service lockers are prohibited.
Broadcasting and Communication Facility	CUP	

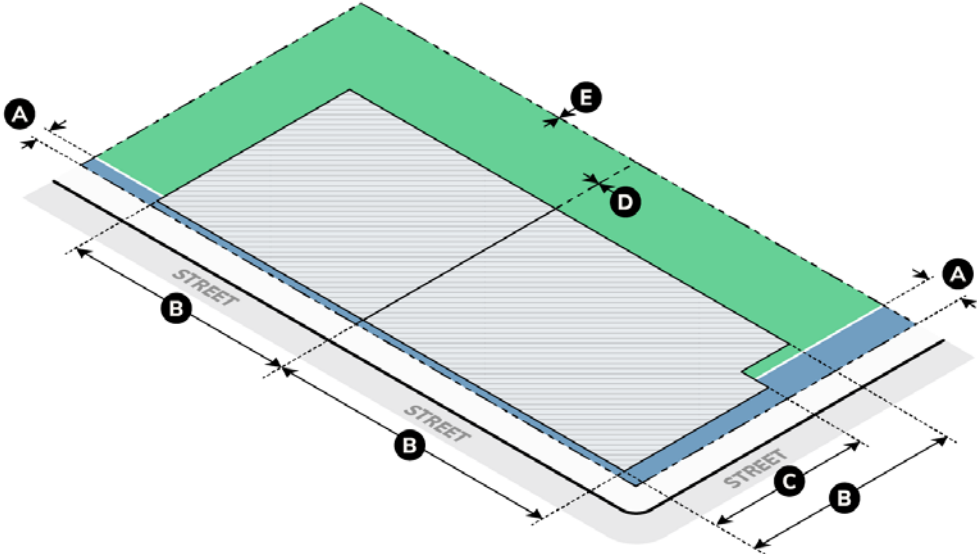
Sec. 3504 - Dimensional Standards

EDITOR'S NOTE: Please review the Staff Report associated with the Draft DTNW District for discussion of dimensional standards in the DTNW District.

(a) Building Siting

The following standards regulate the placement of buildings on lots within the DTNW District. Building siting standards are intended to ensure the creation of an urban development character and a walkable, pedestrian orientation in alignment with the intent of the district, while maintaining compatibility with existing downtown development.

BUILDING SITING



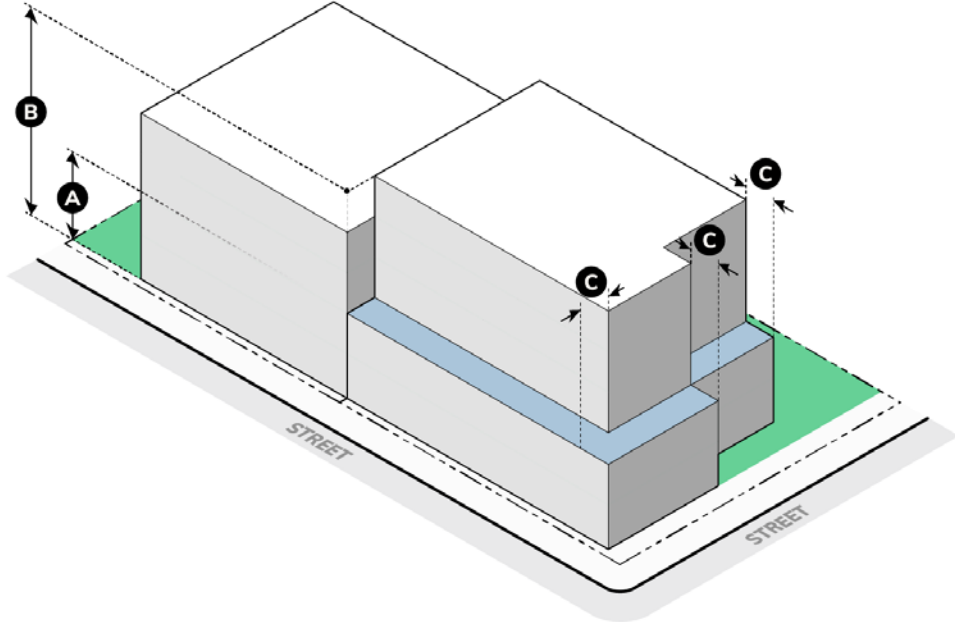
A	Build-To Zone¹	10-20 ft. from back of curb
B	Minimum Build-Out Percentage²	Fronting North Main Street: 80% Fronting any other street: 60%
C	Minimum Build-To Percentage	80%
D	Minimum Side Setback³	None
E	Minimum Rear Setback³	None

¹ Where the width of a public right-of-way exceeds the maximum dimension of a required build-to zone, the right-of-way line shall become a required build-to line.
² If the minimum build-out percentage exceeds a maximum building length requirement, the maximum building length shall control.
³ A minimum setback of 30 ft. shall be provided from any lot line abutting St. Luke and Oddfellows Hall.

(b) Building Height

Building height standards regulate the minimum and maximum heights of buildings within the DTNW District. The intent of these standards is to provide flexibility for the creation of a dense, urban development pattern in the district, while ensuring appropriate transitions and sensitivity to existing downtown development.

BUILDING HEIGHT



A	Minimum Building Height <i>In the case of lots with frontages on multiple streets, the greatest minimum building height controls.</i>	Fronting North Main Street: 30 ft Fronting any other street: 18 ft	
	B Maximum Building Height <i>Applies only to those portions of a structure within the specified distance</i>	Within 120 ft of the centerline of North Main Street	75 ft.
		121 to 250 ft from the centerline of North Main Street	105 ft.
		251 ft or more from the centerline of North Main Street	125 ft.
C	Required Minimum Building Height Stepback	10 ft.	

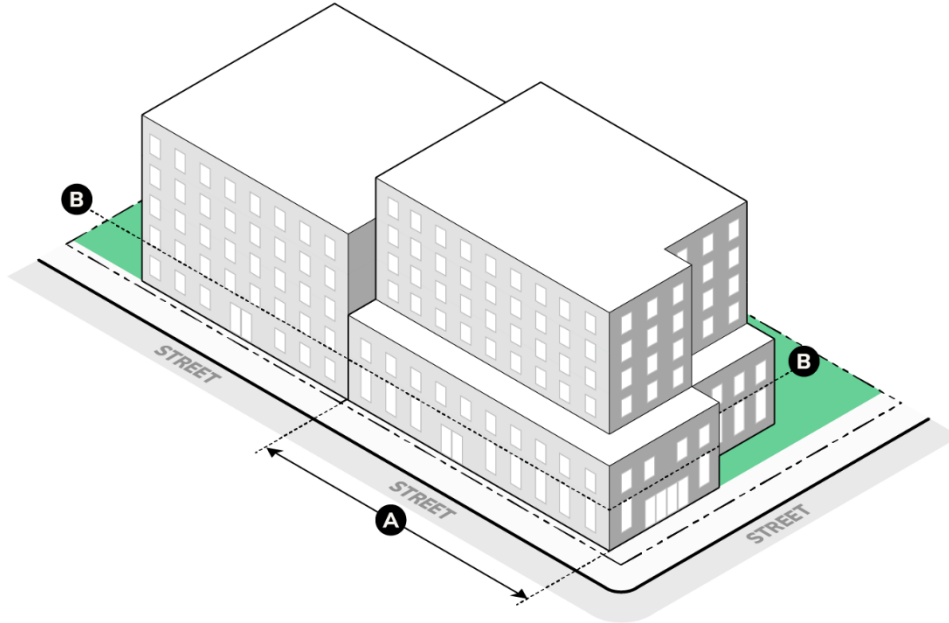
(1) Building Height Stepback

All buildings over 50 feet in height are required to provide a building height stepback. Such stepback applies to any street-facing façade of a building. Stepback shall occur above the second floor, and no higher than the fourth floor.

(c) Building Articulation

Building articulation standards regulate the dimensions of building façades to provide a consistent, comfortable pedestrian experience at ground level. Standards are intended to enhance the pedestrian scale and orientation of the urban environment within the DTNW District by addressing building massing, length, and connectivity.

BUILDING ARTICULATION



A	Maximum Building Length	300 ft.
B	Minimum Ground Floor Demarcation Line <i>This includes a cornice line, first-floor window height, bottom sill of second story windows</i>	Residential (Excluding Lobby): None Nonresidential or Mixed-Use: 14 ft.

(1) Maximum Building Length with Additional Design Features

Maximum building length may be increased to 400 feet with the inclusion of additional design features as follows:

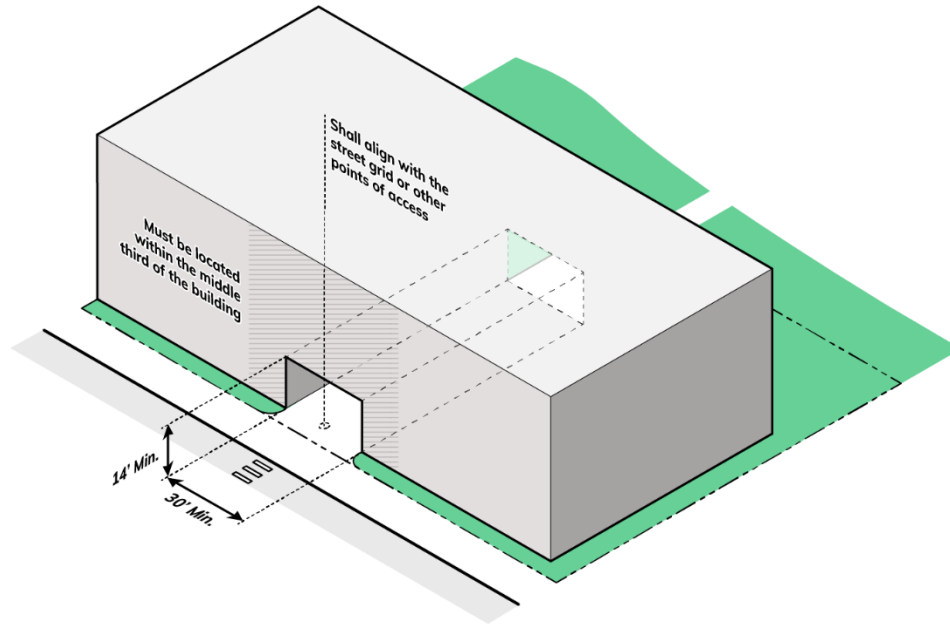
- i. Where a building abuts two parallel streets, or one street and a parking lot, public park, or other public open space on the side of the building opposite the street, a pedestrian passage may be used to achieve additional building length, to a maximum of 400 feet. Such passage shall meet the following criteria:

1. General Requirements.

- a. Passages shall be designed to accommodate pedestrians. Vehicular access and circulation shall not be allowed as a component of a passage.
- b. Passages shall be a minimum of 30 feet in width and 14 feet in height and shall be located within the middle third of the building, measured parallel to the street.
- c. Passages shall be designed to maintain views from one end through to the other. Such views shall not be obstructed by lighting or other design features.
- d. Inclusion of decorative elements such as lighting installations or public art within passages is encouraged.

- e. Passages shall align with the street grid or other points of access to sidewalks, public paths, parking lots, public parks, or other publicly owned open space where feasible.
- f. For the purposes of any build-to zone requirement, a building passage is considered part of the building façade that meets such requirement.

PASSAGE DESIGN



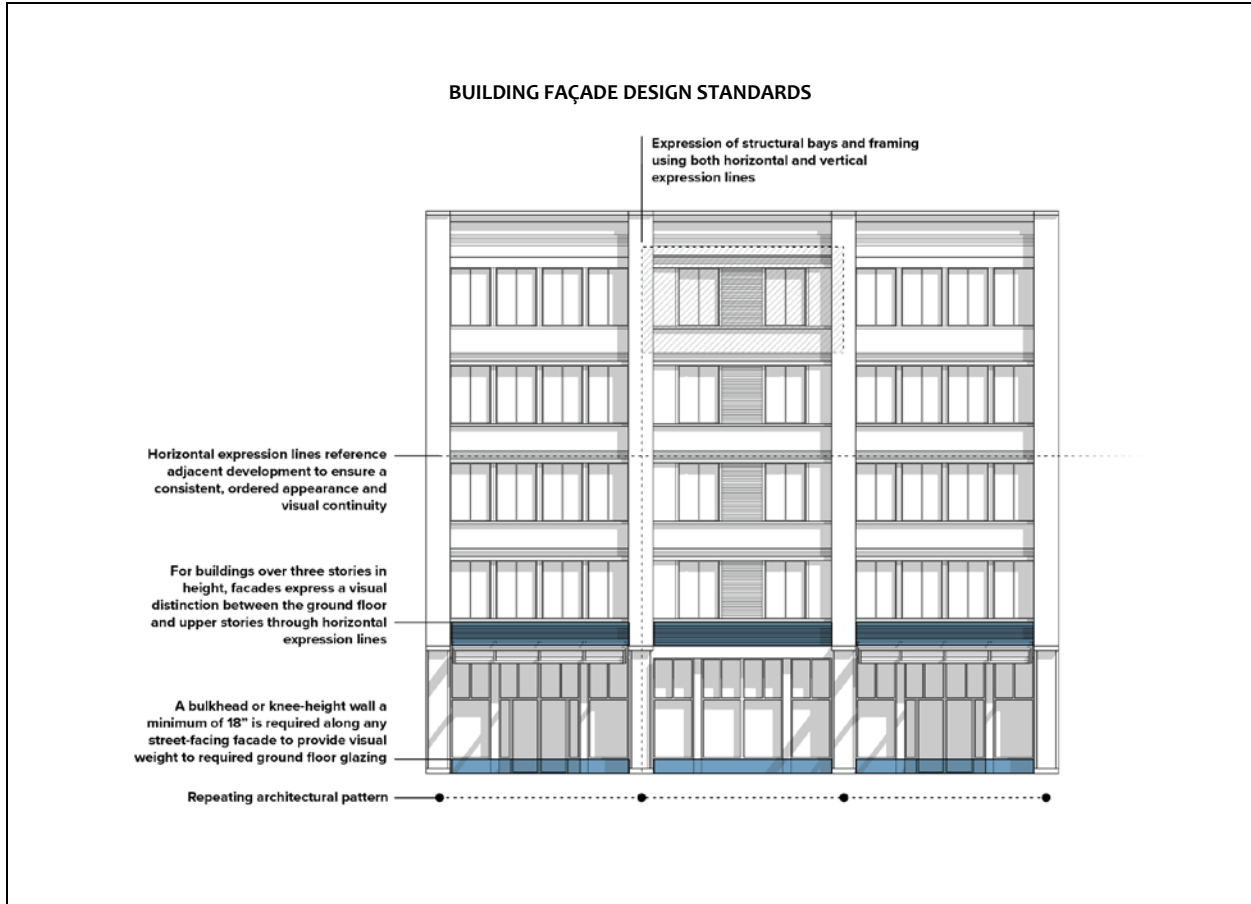
Sec. 3505 – Design Standards

The design standards within this Section apply to all new construction and additions of 400 square feet or more to existing structures. In the case of an addition, only those standards that relate to the specific addition apply.

(a) Table 3505-1: Downtown Northwest District Design Standards, establishes the design standards applicable to frontages that abut North Main Street, and frontages abutting any other street within the District. Where a “√” is present in the table, it indicates that such standard is applicable. A blank, gray cell indicates that the standard does not apply.

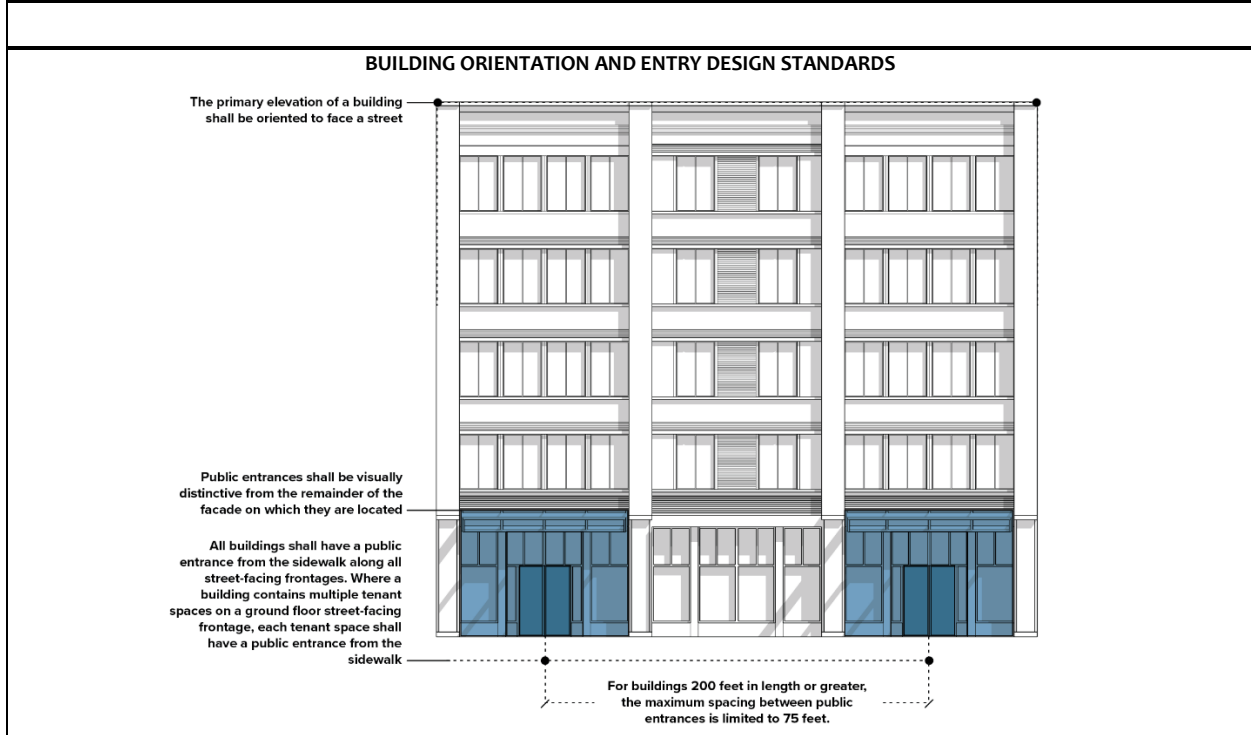
Table 3505-1: Downtown Northwest District Design Standards		
Building Façade Design	North Main Street	All Other Streets
Building façade design standards regulate various components of a building façade to reinforce the DTNW District’s focus on pedestrian experience and high-quality design through the creation of visual interest and a human scale at ground level.		
Building façades shall not contain blank wall areas that exceed 25 linear feet.	√	
Building façades shall not contain blank wall areas that exceed 50 linear feet.		√

<p>Building façades of 100 feet in length or greater shall include architectural variation incorporating a minimum of two of the following elements: color change, texture change, material change, a wall articulation change (such as a reveal, recess, offset, or pilaster), or building recesses or projections. This includes patterns made by structural components visible through glass curtain walls. Such variation shall occur at intervals to minimize the perceived mass of the building façade.</p>	✓	✓
<p>Façades shall express the structural bays and framing of the building using both horizontal and vertical expression lines, to provide interest and detail to the façade. Expression lines may consist of window patterns, change of materials, cornice treatments, or other architectural elements.</p>	✓	✓
<p>Horizontal expression lines shall reference adjacent development to ensure a consistent, ordered appearance and visual continuity along the face of a block. This includes patterns made by structural components visible through glass curtain walls.</p>	✓	✓
<p>A bulkhead or knee-height wall a minimum of 18 inches in height is required along any street-facing façade to provide visual weight to required ground floor glazing. This standard does not apply where residential dwelling units are located on the ground floor.</p>	✓	✓
<p>For buildings over three stories in height, façades shall express a visual distinction between the ground floor and upper stories through horizontal expression lines and architectural features including cornices, recesses, brick courses, or other detailing.</p>	✓	✓
<p>Building materials and visual elements used on the street-facing façade shall be consistently applied to all building façades visible from a street.</p>	✓	✓
<p>Arcades, galleries, colonnades, outdoor plazas, outdoor dining areas, and similar pedestrian-oriented features are encouraged to be incorporated into façades. When provided, such features shall be considered to meet any required build-to percentage.</p>	✓	✓



Building Orientation and Entry Design	North Main Street	All Other Streets
Building orientation and entry design standards require that buildings within the DTNW District are oriented toward the pedestrian realm, and provide legible, predictable points of access along their frontages.		
The primary elevation of a building shall be oriented to face a street. In the case of a building with frontage on Main Street and any other street, the primary elevation shall be oriented toward Main Street.	✓	✓
All buildings shall have a public entrance from the sidewalk along all street-facing frontages. Where a building contains multiple tenant spaces on a ground floor street-facing frontage, each tenant space shall have a public entrance from the sidewalk. This standard does not apply where residential dwelling units are located on the ground floor. The Zoning Administrator may modify or waive this standard if it is determined that, due to site constraints, it is not possible to provide such entrances.	✓	✓
Where a building contains frontage on a public plaza, public entrances may be provided from the plaza. Such entrances are considered to meet the requirement for a public entrance from the sidewalk.	✓	✓
Public entrances shall be visually distinctive from the remainder of the façade on which they are located. Entrances shall contain architectural features that provide weather protection and add visual interest to the structure. All public entrances must be operable.	✓	✓

For buildings 200 feet in length or greater, the maximum spacing between public entrances is limited to 75 feet.	✓	
Where entrances to individual residential dwelling units are located on the ground floor, all such entrances shall be a minimum of eighteen inches above the grade of the adjacent sidewalk to provide a sense of privacy. This requirement may be adjusted or waived by the Zoning Administrator in the case of unique topographical conditions that preclude such designs.		✓



Window and Door Design	North Main Street	All Other Streets
Window and door design standards promote a pedestrian-oriented interface between buildings and the public realm by requiring elements of transparency that allow for visibility both into and out of buildings at the ground floor and above.		
Multifamily Development Transparency: The ground floor of a façade shall maintain a minimum transparency of 70%, measured between two and eight feet in height.	✓	
Multifamily Development Transparency: When the ground floor is occupied by dwelling units, a minimum transparency of 20% of the wall area of the story shall be maintained. When the ground floor contains a lobby or common areas associated with the dwelling, a minimum transparency of 50% shall be maintained, measured between two and eight feet in height.		✓
Nonresidential and Mixed-Use Development Transparency: The ground floor of a façade shall maintain a minimum transparency of 70%, measured between two and eight feet in height.	✓	

Nonresidential and Mixed-Use Development Transparency: The ground floor of a façade shall maintain a minimum transparency of 50%, measured between two and eight feet in height.		✓
Multifamily, Nonresidential and Mixed-Use Development Transparency: The upper floors of a façade shall maintain a minimum transparency of 20% of the wall area of each story	✓	✓

WINDOW AND DOOR DESIGN STANDARDS

Upper floors shall maintain a minimum transparency of 20% of the wall area of each story

Ground floor transparency is required between two and eight feet in height, varying by use and frontage

Roof Design	North Main Street	All Other Streets
Roof design standards ensure the creation of visual interest at the top of buildings, and mitigate impacts of reflective surfaces and/or glare into adjacent buildings.		
Flat-roofed buildings shall feature three-dimensional crowning elements such as cornices to create depth, shadow, and visual interest.	✓	✓
Reflective roof surfaces that produce glare are prohibited, except for high-albedo roofing intended to reduce building surface temperatures and heat transfer into the building.	✓	✓
Site Design	North Main Street	All Other Streets
Site design standards require consideration of pedestrian movement and connectivity both within individual sites and between adjacent development sites, as well as the creation of unified, coherent design character within multi-building development sites.		

Multi-building development sites shall establish a unified design character using common architectural features, building style, building materials, form, scale, and proportion. Such unified design shall carry through all on-site treatments including hardscape, site furnishings, and landscape.	✓	✓
Multi-building development sites shall create legible pedestrian connections between buildings, as well as through the development site using courtyards, plazas, landscape, and walkways.	✓	✓
Development sites shall provide for pedestrian movement to adjacent commercial and residential uses, facilitating connectivity and safe access between the site and adjacent uses.	✓	✓

(b) Restricted Building Materials

(1) The following building materials are prohibited on any façade visible from a public or private street (excluding alleys). However, such materials may be used as a component of construction when not a surface finish material.

- i. Plain concrete block
- ii. Wood structural panel sheathing (e.g., plywood, OSB, particleboard, etc.)
- iii. Plastic
- iv. Vinyl

(2) The following building materials may be used only as decorative or detail elements, limited to a maximum of 25% of the area of each façade visible from a public or private street (excluding alleys). This maximum area may not be combined to achieve more than 25% coverage on any façade. However, such materials may be used as a component of construction when not a surface finish material.

- i. Mirrored glass installed on the ground floor or below any required ground floor demarcation line.
- ii. Mirrored glass installed above the ground floor or any required ground floor demarcation line, unless such mirrored glass is designed to be bird safe. Bird-safe glass is specially designed to make glass a visible obstacle to birds and includes approaches such as fritting, silk-screening, or ultraviolet coating. Patterns shall break up the reflectivity of the glass and be spaced two inches apart horizontally and four inches apart vertically.
- iii. Metal panels, unless they meet the following requirements:
 - 1. Metal panels (flat, textured, corrugated, batten, box rib, etc.) whether standalone, composite, or part of a layered or insulated panel, shall be permitted provided they are part of a complete, manufacturer warrantied cladding system, including integral anchorage to structural supports. Steel panels shall be minimum US Standard 26 gauge.

(3) Exterior insulating finish systems (EIFS) installed on the ground floor or below any ground floor demarcation line is prohibited on all building façades, including those not visible from a public or private street.

Sec. 3506 – Site Development Standards

(a) Off-Street Vehicle Parking

(1) There is no minimum number of parking spaces required for allowed uses in the DTNW District.

(2) When parking is provided within the DTNW District, it shall be subject to the parking maximums established in Section 5220 of this Ordinance. Parking maximums for nonresidential uses are included in Section 5220(b), and may only be exceeded if meeting the requirements of Section 5220(d).

(3) Multifamily dwellings in the DTNW District are not subject to parking maximums.

(b) Location and Design of Off-Street Vehicle Parking

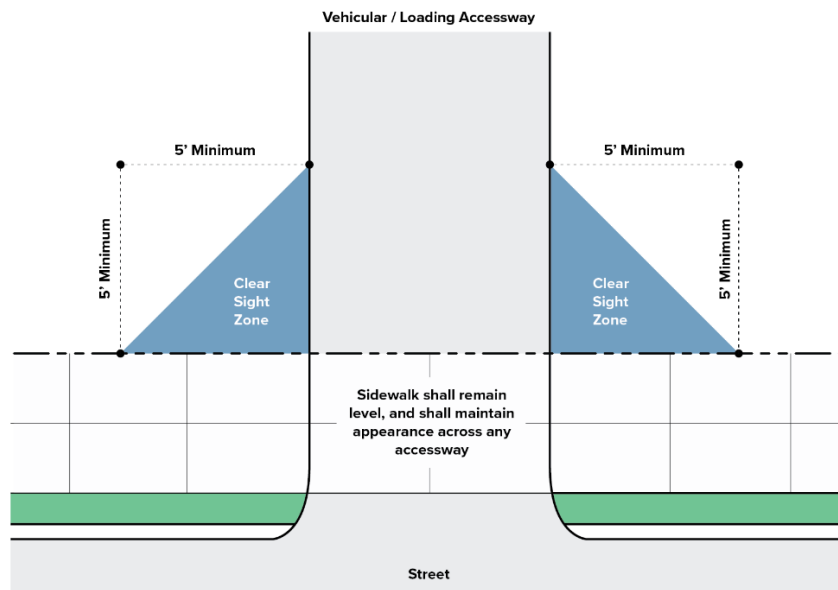
(1) No surface parking, and no drive aisles parallel or roughly parallel to a street may be located between the principal building and any lot line abutting a street.

(2) Vehicular and loading access shall be taken from rear alleyways. This includes access to structured parking facilities. Where alleys do not exist, vehicular and loading access may be taken from a street. In such cases, the following standards apply:

i. Vehicular and loading accessways shall be clearly marked, and shall accommodate a five foot by five foot clear sight triangle to either side of the accessway. Alternatively, exiting detectors may be used in place of a vehicular clear sight triangle if detectors provide signals to both pedestrians and vehicles exiting a structure.

ii. Where vehicular and loading accessways cross a sidewalk, such sidewalk shall remain level, with no change in cross-slope. The appearance of the sidewalk, such as scoring pattern or any special paving, shall be maintained across the accessway to indicate that, although a vehicle may cross, the area remains part of the pedestrian way.

VEHICULAR CLEAR SIGHT TRIANGLE



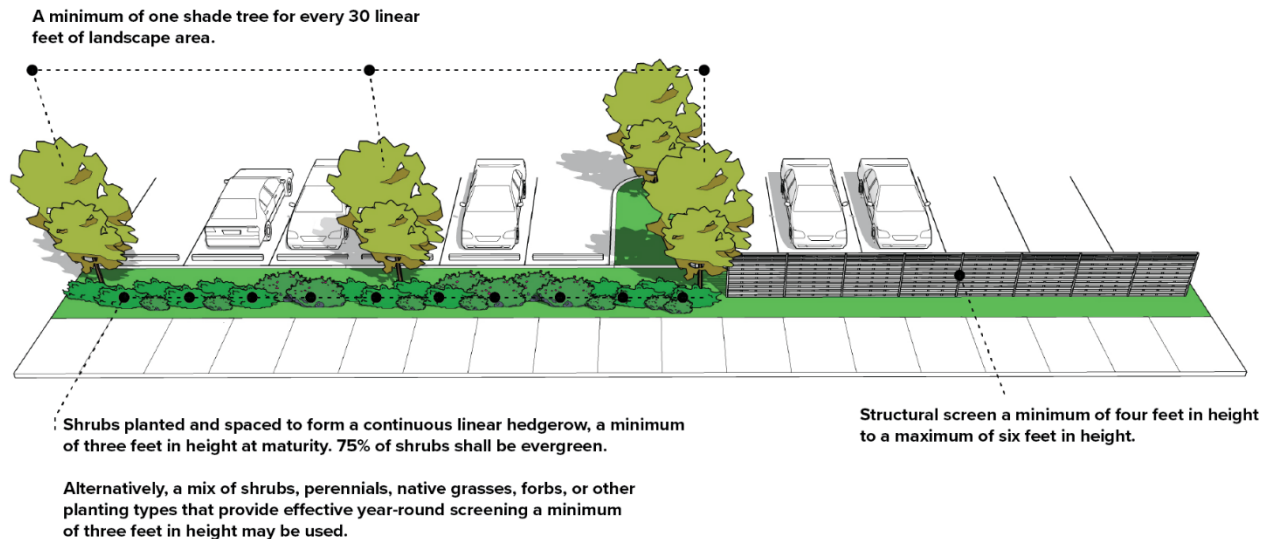
(3) Surface parking lots are discouraged within the DTNW District. Where visible from a street, surface parking lots shall be screened to provide a comfortable pedestrian environment by softening the visual impact of parked vehicles, providing a buffer against glare from headlights, and establishing a landscaped edge at the back of sidewalk. Screening shall be provided as follows:

i. The standards of Sec. 5427 apply to surface parking lots within the DTNW District unless modified by this section. In the case of a conflict, the stricter standard shall control.

ii. Surface parking lots shall be screened by a landscaped area a minimum of 10 feet in width meeting the following standards:

1. Shrubs shall be at least two feet in height at time of planting with an average height of three to four feet expected as normal growth within four years. However, such shrubs shall not exceed four feet in height at maturity. Shrubs shall be planted and spaced sufficiently to form a continuous linear hedgerow at maturity. A minimum of 75 percent of the shrubs planted shall be evergreen species to ensure effective year-round screening.
 - a. Alternatively, a mix of shrubs, perennials, native grasses, forbs, or other planting types that provide effective year-round screening may be used. Such plantings shall provide effective screening to a height of two feet at time of planting, and shall attain an average height of three to four feet expected as normal growth within four years. Such planting shall not exceed four feet in height at maturity.
2. A minimum of one shade tree shall be planted for every 30 linear feet of landscape area. Trees may be spaced in a linear fashion on-center, or may be grouped to complement a design concept. Species shall be selected from the Town's list of recommended tree species (Sec. 5429), and shall meet the standards of Sec. 5420 and Sec. 5429.
- iii. Alternatively, surface parking lots may be screened by a structural element such as a wall or fence that is architecturally and materially consistent with the principal structure. Such structural screen shall be a minimum of four feet in height, and no more than six feet in height, and shall be subject to the provisions of Blacksburg Town Code Section 21-304, "sight triangles established."

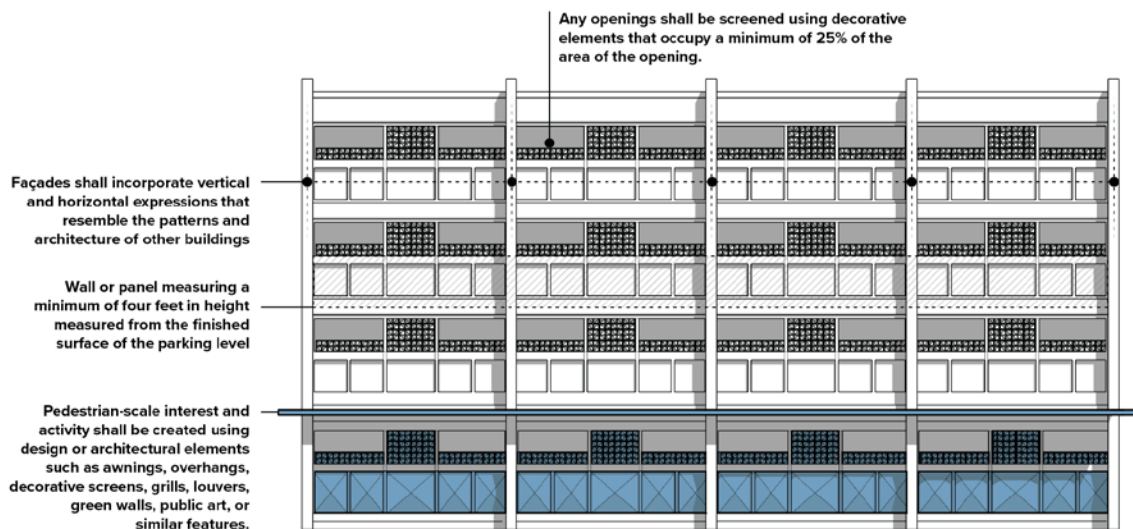
SURFACE PARKING LOT SCREENING



- iii. The Zoning Administrator may modify these screening standards in the following circumstances:
 1. Innovative landscaping or architectural design is employed on the building site to achieve an equivalent screening or buffering effect.
 2. The required screening would be ineffective due to the proposed topography of the site and/or the location of improvements on the site.
- (4) Structured parking is not permitted on the first floor of any building frontage abutting North Main Street, or any building frontage abutting Turner Street NW within 150 feet of the intersection of North Main Street and Turner Street NW. All structured parking located on the ground floor on these frontages shall be screened behind usable interior space measuring a minimum of 30 feet in depth.

- (5) Structured parking visible from a street shall be screened as follows
- i. On the ground floor of a building façade, where indoor usable space, ingress/egress points, and/or mechanical equipment are not present, pedestrian-scale interest and activity shall be created using at least three design or architectural elements such as awnings, overhangs, decorative screens, grills, louvers, green walls, public art installations, or similar features.
 - ii. Parking structures shall be designed to ensure that vehicles parked on all levels of the structure, as well as all interior lighting, are effectively screened by a wall or panel measuring a minimum of four feet in height measured from the finished surface of the parking level.
 - iii. Façades shall incorporate vertical and horizontal expressions that resemble the patterns and architecture of other buildings within a multi-building development, including use of similar materials and a similar pattern of fenestration openings. Any openings shall be screened using decorative elements such as grillwork, louvers, green walls, public art installations, or similar features that occupy a minimum of 25% of the area of the opening. This standard shall also apply to freestanding parking structures that are not within a multi-building development, in which case the façade shall incorporate the patterns and architecture of adjacent or surrounding buildings.
 - iv. For parking structures that include rooftop open-air parking, a parapet wall a minimum of four feet in height measured from the surface of the roof is required.

STRUCTURED PARKING SCREENING



(c) Bicycle Parking

Bicycle parking is a fundamental required element of the DTNW District, and shall be given appropriate consideration from the initial stages of project development. Bicycle parking is required for uses within the DTNW District as follows:

- (1) Bicycle parking is required per Section 5213 of this Ordinance.
- (2) A percentage of the bicycle parking required per Section 5213 must be designed as long-term bicycle parking. All long-term bicycle parking spaces shall be located indoors or beneath a full permanent cover such as an overhang, covered walkway, or outdoor bicycle locker. Where long-term bicycle parking is not located within a building or a locker, any cover used to meet this standard shall be of permanent construction and able to withstand rainfall, snow, or other inclement weather conditions. Any covered area shall maintain a minimum vertical clearance of seven feet from grade to the bottom of the cover.

- (3) Long-term bicycle parking spaces are required in accordance with the following:
 - i. Residential units: 60% of bicycle parking spaces required per Section 5213 shall be long-term spaces.
 - ii. Commercial retail: No long-term spaces required.
 - iii. Civic, office or any other use not listed above: No long-term spaces required.

(4) The Zoning Administrator may approve alternative compliance with the requirements for long-term bicycle parking spaces upon finding that a modification would be consistent with the purpose and intent of this Section, and the purpose and intent of the Town’s Comprehensive Plan. Alternative compliance with these requirements must be approved at the time of rezoning to the Downtown Northwest District.

(d) Drive-through Facility Location and Design

EDITOR’S NOTE: Please review the Staff Report associated with the Draft DTNW District for discussion of drive-through facilities in the DTNW District.

Drive-through facilities are allowed within a limited area of the DTNW District, and are subject to Sec. 5230 of this Ordinance, as well as the following standards.

- (1) Drive-through facilities are permitted only on lots fronting Turner Street NW within 350 feet of the intersection of North Main Street, measured parallel to the curb line. When located within the first 200 feet of the intersection of North Main Street, drive-through facilities are only permitted for drive-in restaurants located on the ground floor of a multi-story mixed-use building.
- (2) Drive-through facilities, including any required stacking spaces and drive-through lanes, shall not be located between the principal building and any lot line abutting a street.
- (3) Building façades adjacent to any drive-through lane shall be articulated to ensure visual interest through variation in depth, direction, and design elements. Such façades shall include architectural elements and materials consistent with the overall design of the structure, and should use landscape elements, green screens, art installations, and similar features to soften the appearance of blank walls.
- (4) Any drive-through facility adjacent to a street, including stacking spaces and drive-through lanes, shall be screened in accordance with the standards for the screening of off-street parking lots (350_. (b)(3)).

(e) On-Site Open Space

On-site open space is required within the Downtown Northwest District. Such open space is intended to provide opportunities for gathering and enjoyment while maintaining the district’s unique urban character, and may be designed as public open space, common open space, or private open space as established in item (1) below.

(1) On-Site Open Space Required

- i. New development within the DTNW District is required to provide on-site open space, except for development on sites of one-half acre or less in size.
- ii. Driveways, parking areas, bicycle parking areas, above ground utility areas, mailboxes, and required screening or buffer areas shall not be counted toward on-site open space requirements.
- iii. Development shall provide a minimum amount of on-site open space as follows:

	Lots Fronting North Main Street	Lots Fronting Any Other Street
Total On-Site Open Space Required	5% of lot area	10% of lot area

	Lots Fronting North Main Street	Lots Fronting Any Other Street
Civic, Office, and Commercial Development	50% of total required on-site open space shall be public open space. The remaining 50% may be designed as public open space, common open space, or any combination thereof.	
Mixed-use Development	25% of total required on-site open space shall be public open space. The remaining 75% may be designed as public open space, common open space, or any combination thereof.	
Residential Development	N/A	100% of total required on-site open space shall be designed as private open space, common open space, or any combination thereof.

(2) Design of On-Site Open Space

- i. On-site open space is classified into three categories within this Section:
 - 1. Common Open Space: Open space maintained for the shared use of residents and/or tenants of a development.
 - 2. Private Open Space: Open space reserved for the sole use of the occupant of the associated dwelling unit or tenant space.
 - 3. Public Open Space: Open space maintained for the use of the public. Public open space may include plazas, parks, and public seating areas.
- ii. Required open space shall meet the design requirements of Table 3506-1: Design of On-Site Open Space. Where a “✓” is present in the table, it indicates that such standard is applicable to the indicated type of open space. A blank, gray cell indicates that the standard does not apply.

(3) Exemptions from Required On-Site Open Space

- i. The following uses are not required to provide on-site open space: public parks and recreational areas, minor utility services, and safety services.
- ii. The Zoning Administrator may waive the requirement for public open space, or may waive the requirement for all on-site open space if they determine it is incompatible with the nature of the use.

Table 3506-1: Design of On-Site Open Space			
Design Requirement	Public	Private	Common
Open space shall be located outdoors or in the open air (under a roof, canopy, or screened).	✓	✓	✓
Open space may be located on the ground, or on decks, galleries, porches, terraces, patios, or roofs.	✓	✓	✓
Open space may be located on balconies.		✓	
Open space shall have a minimum dimension of ten feet on each side.	✓	✓	✓

When located on a balcony, porch, deck, or roof, open space shall have a minimum dimension of ten feet on one side and seven feet on the other.		✓	
A minimum of 25% of total required open space area shall be contiguous.	✓		✓
Open space may abut a parking lot on one side only.	✓	✓	✓
Open space shall be located a minimum of 15 feet from loading spaces or service areas.	✓	✓	✓
When located at ground level, the required open space area shall be substantially covered in a combination of at least two of the following: 1) grass and groundcover; 2) shrubs and trees; or 3) usable outdoor hardscape features, such as courtyards, seating areas, patios, fountains/water features.	✓	✓	✓
Circulation within the open space area shall connect pedestrians to rights-of-ways that abut the open space, entrances to adjacent buildings, and any design features, such as seating areas. Circulation paths are considered part of open space.	✓		
Outdoor amenities, such as grills, pools, tennis courts, or playgrounds, are permitted as part of the required open space		✓	
Where possible, open space should be connected to abutting public parks and greenways in coordination with Blacksburg Parks and Recreation.	✓	✓	

Sec. 3507 – Signs

The character of the Downtown Northwest District – a dense, urban, well-connected, mixed-use area encouraging variety in architectural forms – requires special consideration to ensure that signs within the district reinforce and complement its unique orientation. Signs within the district may require additional flexibility to meet the needs of this type of development, anticipated to include a mix of sign types such as individual tenant signs in multi-tenant buildings, building-mounted signs, monument signs, and others. To ensure that signs in the DTNW District complement the intent and character of the district, the following is required.

- (a) Creation of a special signage district is required, in accordance with Section 5540 of this Ordinance.
- (b) Application for a special signage district, in accordance with Section 5540(d) shall be reviewed and approved concurrently with a rezoning to the Downtown Northwest District.